

The SMP saddle has revolutionized the way people think about bike saddles. The cyclist is at the centre of the project and ergonomic studies have analyzed his/her positions during the race and the interaction of the various body parts with the saddle.

Ho acquistata recentemente una delle vostre selle ergonomiche SMP, e sono rimasto I have recently bought one of your SMP ergonomic saddles and I have been really satisfied. I have been mountain- and road-biking seriously for a long time and have tried a great number of saddles. However, your SMP saddle has proved to be by far better both as far as ergonomics is concerned and for its performance while pedalling. It is positively a better designed saddle. In particular, it should be noticed that your saddle allows the pelvic bones to be less angled, which, first of all, alleviates the load on the back and secondly offers a better position while pedalling. Simply by using your saddle, a biker can keep a lower position more easily while pedalling.

Sebastian B. (Germany)

I bought an SMP Evolution saddle and I wish to thank you for the well-being that I received from it, since with my other saddle, even if it was fitted with a central hole, I experienced pins and needles, irritation and sometimes burning while urinating. Now on the other hand, not only I don't have any problems, but biking has become a pleasure and I like the saddle even from an aesthetic point of view.

Luca S. (Novara - I)

Back to racing at good levels? I would have been satisfied with going back onto the saddle for a few kilometres with my friends. Instead I've gone back to racing and winning. Even my urologist was sceptical: the diagnosis had been hard and unambiguous but the treatment has been effective and the saddle goes on performing its "small miracle" day after day. Thanks SMP, thanks for allowing me to go on racing without endangering my health.

Mauro T. (Reggio Emilia - I)

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All the models in the SMP range have the same patented characteristics in common, such as the original beak of the saddle nose, designed to provide a flat base for the efforts while sitting during long climbs and as a relief during the descents, offering better control of the bike with the thighs.



I have tried it on the flat and it "ties" with the pelvis very well, I had the sensation of not having replaced the dear old saddle that I had been using for ten years.

I've had the sensation of it being very comfortable and it seems to offer brilliant anchorage while pushing (I rarely get up on the saddle, above all while climbing, because I need a good supporting base to express full power, above all in the toughest climbs).

Luigi G. (Ascoli Piceno)

Since the middle of March I have done about 1,000 km with SMP, doing trips of over 200 km. The saddle is superb for sports racing, which means that the necessary pressure is applied to the pedal, unloading the "bottom". This means the cyclist does not rest with his whole weight on the saddle and can sit for eight hours and even longer. The area of the genitals does not become insensitive. The shape of the saddle leads the cyclist to an aerodynamic position, which means that it feels more comfortable to grip the lower horn.

Stephan S. (Germany)

After covering about 500 miles with your Full Carbon saddle, I wish to compliment you since, apart from being aesthetically beautiful, it offers original characteristics such as the possibility of leaning on the nose when you push forward during the efforts on long climbs.

Frank J. W. (Riverside - CA)

The particular shape of the SMP saddle is not only ergonomic; the original profile and its soft lines make it particularly elegant. A component that improves increasingly more sophisticated bikes. The wide range of colours allows innumerable colour combinations.

I fitted the Carbon saddle onto my ultra-light bike and I'm writing to you to confirm that the effect is really superb. In profile, the saddle looks like an F1 car and this gives the bike an even more elegant appearance.

Robert D. (Edinburgo - GB)

My mechanic recommended that I fit your Composit saddle onto my new bike. I have to thank him because the saddle, covered in yellow leather, is wonderful. But the real surprise was after using it. Pins and needles disappeared and my blood seemed to run through my legs quicker.

Luciano M. (Vicenza - I)



The length of the bars makes installation particularly easy on practically any combination of seat frame/ upright.

Once the right position on the lug is found (I haven't even used a spirit level since it was so easy to make it level) you feel the glutei rest on the bone and not on the muscle.

Gabriele C. (Berna - CH)

The initial adjustment suggested in your instruction booklet seemed to give already unexpected results for people, like me, who have had very serious accidents (fracture of the pelvis).

The length of the fork has allowed me to adjust the saddle further very easily so that the position could be adapted to my height: 190 cm.

Ferruccio B. (Grosseto - I)

The central channel in the saddle is an uncommon characteristic because of its size: almost a whole hand can pass through it and it allows anus, prostate, pudendal veins, deep dorsal penis artery and vein, scrotum and testicles for men and small and large lips and clitoris for women, not to be squashed. Air can circulate also around the private parts and this prevents that unpleasant hot sensation.



Right from the start when I sat on it (for just a few metres in the garage of the company I work for and dressed for work) I had the wonderful sensation of resting my private parts literally on an empty space. In the following days I covered a few hundreds kilometres and I must really admit that it is a noticeable improvement.

Mariella L. (Naples)

I'm sending you my impressions after my first two outings.

Saturday: a little over 70 km all on the flat to cover with the usual gentle-fast rhythm (85-95 thrusts on the pedals); great comfort for my privates, no pins and needles/tingling in the toes of my left foot that normally arrived a couple of hours later.

Sunday: 85 mixed km (700 metres' total difference in level), covered with variable rhythm: there seem to be no problems with my entrails.

Emanuele S. (Rome)

I'm writing to you to compliment you on your saddle. Apart from having no more problems with the compression of my privates that, for us women, are particularly annoying, I get the distinct impression of being fresher.

Martina R. (Mastricht - NL)

Next year will be my third as a professional and I can say I've had a lot of problems with saddles in the past.

I tried a lot of different types, but none really helped me.

Today I've raced for the first time with an SMP saddle for over three hours and I've had no problem whatsoever!

Kevin V. den S. (Belgium)

A cyclist sits on an SMP saddle with his/her glutei and the lower part of his/her pelvis (ischiatric tuberosities) and the position is determined by the shape of his/her bottom that, apart from preventing crushing the coccyx on rough terrain, allows soft standing and sitting from the back when the terrain requires it.



I have spent literally hundreds of Euros on saddles over the last few years, all trying to find one that works for my body shape - all with no luck.

My problem is that after about 20 minutes riding on the drops, my legs go numb, and I get pins and needles.

This has been a big issue for me and has stopped me performing as well as I could.

Last month I read about the Selle SMP Plus, and thought it would be worth a try.

Last night I tried it for the first time on my usual ride. The difference it's made has been enormous.

Cycling is now a pleasure - and not a pain, because of your saddle.

I can now stay down on the drops for 10, 20, 30 kilometres of cycling without any discomfort at all.

Thanks again.

Andrew H. (Oxford - GB)

I bought the Stratos model and I am really happy with it. I have already covered over 1,000 Km in MTB on all types of terrain and the particular shape of the back allows, in my opinion, a particular comfort on slopes, apart from making standing and sitting easier. The width of the central channel guarantees aeration and, contrary to other saddles, I don't get the annoying hot sensation in my privates.

Sandra B. (Rotenburg - D)

The difference I noticed from "traditional" saddles was massive and I performed some tests.

1 h 30 min on rollers (40 Km) - LONG-DISTANCE test

Unlike traditional saddles, the SMP saddle has never needed any lifting

2 h 00 min on rollers (50 Km) - SFR test + 100 rpm agility

I have noticed a great advantage in the agility tests where, because of the high number of thrusts on the pedals (100 a minute), the undersaddle undergoes more stress. I could quantify that during the hour's thrusting on the pedals with this kind of specific work, with traditional saddles 4 lifts were necessary, while with the SMP saddle only one was required.

3h 30 min on rollers (about 90 Km) + outing with carbon race bike 1h 30 min (45 Km).

Here I had the most surprising results. During the two tests, only a couple of lifts were needed and anyway they were NOT caused by soreness of the genitals but by a problem I have (a cyst in the ischiatic area) and the most surprising thing at the end of the thrust on the pedals was that there was no tingling or anaesthetic effect in the genital area and, apart from a light tiredness in the legs, it seemed nearly as if I had not spent any time on a bike at all.

1h 35 min (40 Km) - outing with my mountain-bike on a tarmac and undulating route.

The use of the STRIKE saddle made the run comfortable and confirmed that, with time, the initial sensation of greater pressure in the supporting area disappears very quickly.

Mauro F. (Verona)



It was users who highlighted that the SMP saddle is particularly good also on standing and sitting at the front, allowing comfortable use also to people competing in extreme biking with wide shorts.

As you recommended, I have purchased a Glider saddle for my MTB.

My first impression was very good; after fitting it personally following your clear instructions, I covered 79Km alternating tarmac and dirt road without experiencing any discomfort or tingling. If my initial impression is confirmed, I will recommend your saddles to all my BIKER mates.

Marco S. (Pescara)

I saw your saddle in a shop in Berkley and I immediately thought its shape was ideal for downhill fans. The test exceeded my expectations. With long shorts it is possible to stand and sit very comfortably, without any problem between the trousers and the saddle.

Its lowered nose prevents the accidental contact with the genitals and its open shape on the back prevents impacts on the coccyx.

Billie K. (Berkley - CA)

The saddles are manufactured wholly in Italy by Selle SMP, a company formed 60 years ago and therefore a pioneer of the sector in the world.

The quality of the leather and the other materials and the accuracy of the work are guaranteed even today by the passion the Schiavon family transmits from generation to generation.

As promised, I wish to let you know that, after covering 1500 km, the condition of the Evolution saddle is very good. I will actively promote your products and shall continue to support their superb quality in first person.

Franco D. (Lecco)

The cover leather is superb, soft and it dries quickly when it gets wet; the embroidered name does not bother in the least while pedalling, even if it is slightly in relief.

Marcello F. (Genova)

Many thanks.

I wish to let you know that I called your company and I wanted to congratulate you for your kindness, availability and knowledge.

Leonardo A. (Milano)

Many thanks again for the punctuality, precision and friendliness shown while giving me all the necessary information regarding the technical specifications and the prices of the different models in your range.

If your products match your "pre-sale" kindness and professionalism I think it will be difficult for me to find anything better on the market.

Luisa S. (Salerno - I)

I must congratulate your Company. I had some problems with the area resting on the saddle, in fact last Sunday, after 20 km in the race, I wanted to stop for that reason but, clenching my teeth, I had to get up on the pedals frequently because of the pain to finish the race.

On Monday I decided to buy an SMP saddle (for an amateur it's not easy to replace a saddle or any other component when you've been using it for ages). I trained without any problems and I was so happy with it that I coined the saying:

"The most delicate spot for a cyclist rests on an SMP saddle".

I thank you on behalf of my "delicate spot".

Eugenio G. (Treviso)



## What the Physicians say

Abstract from: "Journal of Sexual Medicine"

### A cycle saddle with a new geometrical conception for maintaining the vascular perfusion of the genital-perineal region

Breda G. and Lunardon E.: Urologia Hospital S. Bassiano, Bassano d.G.

Piazza N.: Urologia Hospital SS. Giovanni and Paolo, Venezia

Bernardi V.: Anaesthesia and Reanimation, Hospital S. Bassiano, Bassano d.G.

**Introduction:** An increase in the incidence of the erectile dysfunction of the penis was observed in those cyclists that cover long distances. All the theories converge on the identification of the perineal region as the critical point.

**Objectives:** Identification of a bicycle saddle model suitable for cyclists covering long distances, capable of limiting compression of the structures of the pelvic floor, and thus protecting the blood perfusion of the penis and preventing possible consequences in erection.

**Materials and methods:** comparison between the new geometrical conception saddle (SMP) and one of the models most frequently used by professional cyclists.

Assessment of the degree of compression exerted by the two saddles, on the vascular structures of the perineum, by measuring the partial pressure of the transcutaneous oxygen of the penis in 29 healthy, cyclist volunteers.

Readings of the transcutaneous O2 pressure carried out after 3 and ten minutes of static seating. Subsequently the values of the PtCO2 were determined for 15 minutes while pedalling with the subject at 60° and in stable haemodynamic conditions.

**Results:** Data analysed statistically (test t Student, examination of the confidence interval). Demonstration of the net superiority of the SMP saddle in preventing vascular compression of

the perineal structures with statistically important results.

The true innovation brought about by the SMP saddle is its capacity of interfering scarcely on the blood perfusion of the penis, maintaining limited dimensions, especially in width, parameter considered to date as an essential factor in the protection of the compression on the perineal structures.

It is the geometry of this saddle that, by redistributing the body weight onto the buttocks, the ischial tuberosities, the ischium freeing the perineal plane, prevents the squashing of the neurovascular structures that run medially to the ischial tuberosities. Furthermore, the depression in the rear part prevents the coccyx from touching the saddle, thus preventing repercussions caused by the unevenness of the ground that may affect the spine.

**In our study, the SMP saddle has shown to be by far the most efficient in protecting the blood perfusion of the penis** compared to one of the saddles most frequently used by professional cyclists.

**Conclusions:** validation of the efficacy of the SMP saddle in limiting the compression on the pelvic floor even though it has dimensions, especially in width, compatible with the needs of the cyclists that cover long distances.

on your body

In the SMP saddle the cyclist's sitting position is spread out between the buttocks and the lower part of the bone of the pelvis (the ischiatic tuberosities).



The front part, characterised by the original eagle's beak shape avoids the genitals from being squashed and offers a **good supporting base** in the characteristic thrust pedalling.

## Clothing

### Central opening: the Smp4bike novelty

You can finally satisfy your physiological needs without the stress caused by undressing. The central channel of the Smp4bike saddle range allowed us to develop an innovative pair of shorts with a zip in the lower part. The zip neither rubs on the saddle nor constricts your private parts: you only realize it's there in the moment of need ....

### Material: Performer

It's a stretch, strong, easily maintained and very comfortable material. These characteristics offer the right protection, comfort and well-being in sport.

### Bottom: SmpTech

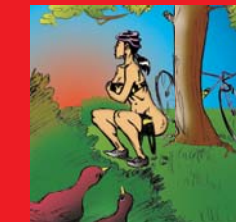
Created by the union of various materials, SmpTech is the new seamless bottom. This allows the cyclist to diversify and distribute his/her weight on the saddle better. The central part of the bottom is divided into two parts, more thickly padded. Once matched and perforated, it increases the transpirability and the transfer of the heat towards the outside. Comfortable even after many washes, its performance remains unchanged.



**smp4bike**  
more than a saddle!

**smp4bike**  
designed

**smp4bike**



NO



YES! IT'S 4 BIKE



NO



YES! IT'S 4 BIKE

## LeatherProtector

Leather Protector is the natural ally of saddles in leather. Leather Protector improves the resistance to friction and protects leather by creating an invisible transpiring barrier that makes leather resistant to water, oil, protein and alcohol spots.

The surface becomes easier to clean increasing the life of the saddle, extremely simple: after cleaning the saddle, rub it with Leather Protector.

Then the leather can be polished with a dry cloth.

It is suitable for all types of leather, nabuck and suede excluded.

Leather Protector is part of the smp4bike range by Selle SMP, a company formed in 1947 and therefore one of the pioneers of the sector in the world.



Via Einstein,5  
35020 Casalserego (PD) - Italy  
Tel. +39 049 643966 - Fax +39 049 8740106  
info@sellesmp.com  
ISO 9001:2000 certified

SELLE  
**SMP**  
1947-2007

www.sellesmp.com

GB

The width of the central channel allows the **private parts** that are normally in contact with the saddle to be ventilated.

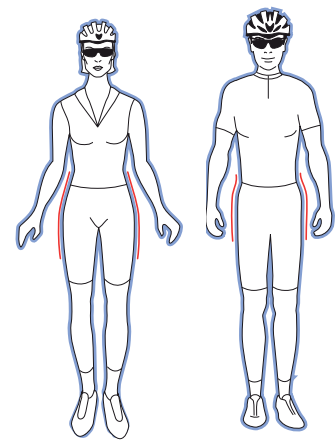


In the lower part of the saddle there is a concavity that prevents the coccyx from touching the saddle, thus **avoiding the counterblow** caused by uneven ground.

The central channel of the saddle, 3 to 5 cm wide, prevents the anus, the prostate, the pudenda veins the deep dorsal vein and artery of the penis, scrotum and testicles, and the labia majora and minora, and clitoris, not to be squashed at all, **allowing a continuous and completely natural blood**.



SELLE  
**SMP**  
1947-2007  
edition 2010



## full carbon

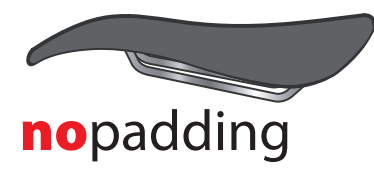


Casing: carbon fibre  
Frame: carbon fibre  
Dimensions: 263 x 129 mm  
Weight: 105 gr

## carbon



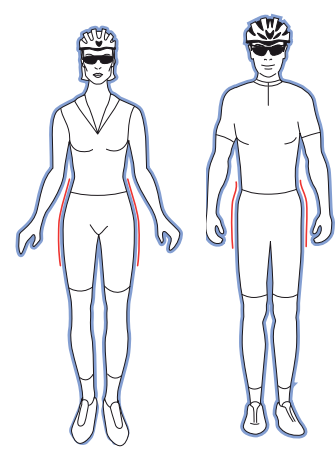
Casing: carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 263 x 129 mm  
Weight: 165 gr



## composit



Covering: nap leather  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 263 x 129 mm  
Weight:  
composit **carbon rail** 200 gr  
composit **carbon rail** 150 gr



## new carbon Lite



Casing: carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 273 x 135 mm  
Weight: 175 gr



## new forma



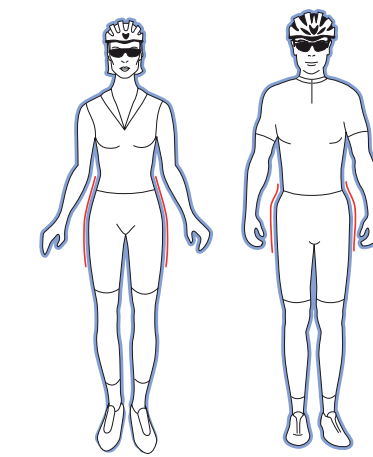
Covering: nap leather  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 273x 137 mm  
Weight:  
forma **carbon rail** 230 gr  
forma **carbon rail** 170 gr

## carbon color

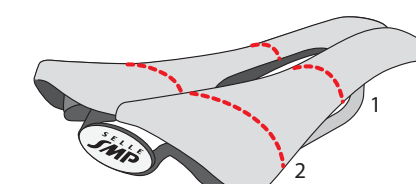


## colors

available in these colours:  
composit, forma, evolution, stratos,  
glider, Lite209, avant, pro, plus:



## evolution



Covering: nap leather  
Padding: foamed elastomer  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 266 x 129 mm  
Weight:  
evolution **carbon rail** 230 gr  
evolution **carbon rail** 180 gr

## stratos



Covering: nap leather  
Padding: foamed elastomer  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 266 x 131 mm  
Weight:  
stratos **carbon rail** 250 gr  
stratos **carbon rail** 200 gr

## glider

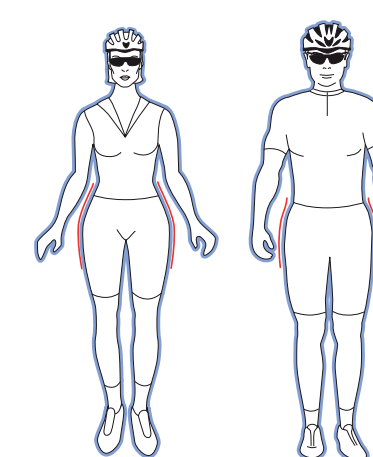


Covering: nap leather  
Padding: foamed elastomer  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 266 x 136 mm  
Weight:  
glider **carbon rail** 260 gr  
glider **carbon rail** 210 gr

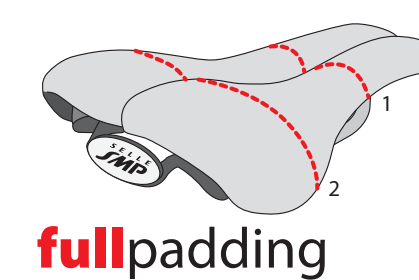
## Lite209



Covering: nap leather  
Padding: foamed elastomer  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 273 mm x 139 mm  
Weight:  
Lite209 **carbon rail** 310 gr  
Lite209 **carbon rail** 260 gr



## avant



Covering: nap leather  
Padding: foamed elastomer  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 269 x 154 mm  
Weight:  
avant **carbon rail** 335 gr  
avant **carbon rail** 285 gr

## pro



Covering: nap leather  
Padding: foamed elastomer  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 278 x 148 mm  
Weight:  
pro **carbon rail** 315 gr  
pro **carbon rail** 265 gr

## plus



Covering: nap leather  
Padding: foamed elastomer  
Casing: nylon 12 charged with carbon fibre  
Frame: AISI 304 tubular steel  
Dimensions: 279 x 159 mm  
Weight:  
plus **carbon rail** 360 gr



## carbon rail

The new "Carbon Rail" frame, available for the Composit, Forma, Evolution, Stratos, Glider, Lite 209, Avant, Pro, models and all the Lady versions, is made in carbon unidirectional fibre. This allows not only a considerable saving in weight than the stainless steel AISI 304 frame, it also offers better performance as well as superb comfort thanks to the huge flexibility of the composite material with a high level of vibration absorption.



## Lady line

### composit Lady



### new forma Lady



### evolution Lady



### stratos Lady



### glider Lady



### Lite209 Lady



### avant Lady



### pro Lady



### plus Lady



Regina Schleiker wins with Lady line

